



atlas

Existing Conditions

TEXAS A&M - SAN ANTONIO AREA
REGIONAL CENTER PLAN



TORRE DE ESPERANZA

TEXAS A&M UNIVERSITY
SAN ANTONIO

STOP

SA Tomorrow Project Overview

By 2040, Bexar County's population is expected to increase by approximately 1 million people, 500,000 new jobs, and 500,000 new housing units. The uncertainty and complexity associated with planning for the next 25 years is daunting. However, San Antonio is planning boldly. We're tackling the tough issues and making the hard choices because "business as usual" isn't good enough. We're planning now to ensure that our great city captures the type of growth and economic development that is compatible with our community's vision of the future, and provides benefits to all our current and future residents. SA Tomorrow was established to implement the SA2020 vision, and includes three complementary plans: the updated Comprehensive Plan, the Sustainability Plan, and the Multimodal Transportation Plan. These plans all work in concert to guide the city toward smart, sustainable growth.

The SA Tomorrow plans prepare our community for the anticipated population and employment growth and to help us understand what that growth will look like and how it will affect our daily lives. With a relatively fixed area available for future development, the anticipated population and employment growth will certainly have an impact on our community's overall quality of life and livability. We also have to ask ourselves if it's now time to expand our boundaries or focus on development within the city's existing footprint. To be successful and truly address the long-term issues facing San Antonio, we as a community, need to tackle the difficult questions that arise from an honest assessment of our community's challenges and clearly state the hard choices we must make to achieve the community's vision for the future. Many of these hard choices are rooted in the fact that current trends have resulted in systems and development patterns that are unsustainable or that produce results counter to our community's stated vision and goals.

Reversing decades-old habits and changing entrenched systems is difficult. The three citywide SA Tomorrow Plans started the process for San Antonio. Now, we are working in more focused geographies across the city to implement the policy recommendations and growth patterns called for by SA Tomorrow. These Sub-Area plans — including Regional Centers, Community Areas, and Corridors — will address a range of issues such as land use; transportation and mobility; parks, trails, and open space; housing and economic development strategies; infrastructure; and urban design. This Atlas helps catalogue the most important existing conditions relevant to the Texas A&M - San Antonio Area Regional Center Plan. Along with extensive community input, best practices research, and expert technical analysis and advice, these findings will serve as an important input to plan recommendations and implementation and investment priorities.



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Citywide Vision and Plans

SA2020

The SA2020 vision originated with a series of public forums in 2010 to develop goals for improving San Antonio by the year 2020. The process was a community-wide visioning effort guided by a steering committee of community leaders and representatives. In addition, thousands of San Antonians participated in the visioning process, which culminated in a detailed report, released in 2011, that outlined a bold strategic vision for San Antonio's future. The SA2020 vision provided a significant foundation for the three SA Tomorrow plans, reflecting the community's desire to support economic development and new jobs while fostering community arts, education, health and culture.



VIA's Vision 2040

Vision 2040 was a community-driven process to update VIA Metropolitan Transit's Long Range Comprehensive Transportation Plan through the year 2040 and develop our region's vision for the future of public transportation. The Vision 2040 planning process occurred alongside SA Tomorrow and identifies a range of transit solutions to serve our region's busiest and most vibrant areas of activity, employment and housing. The plan presents various modes of transportation, and develops system alternatives to understand how transit could affect our region. By engaging the community, Vision 2040 will work to evaluate all alternatives and identify a preferred system plan that meets the transit needs of today and tomorrow.



SA Tomorrow Comprehensive Plan

The SA Tomorrow Comprehensive Plan addresses wide-ranging and interconnected citywide issues. As a long-range vision for San Antonio, the Plan provides strategic direction for decision making and community investment. Perhaps the most important task tackled by our community when crafting this Comprehensive Plan was determining where growth should be directed and encouraged, and doing so in a way that protects vital historic, cultural, social and natural resources.

If guided properly, the expected influx of new residents and jobs will enhance our city and all our residents. Planning now allows us to direct growth in a manner consistent with the community's vision and our goals for the future. The Comprehensive Plan provides an overarching framework for the physical form of San Antonio. It outlines how regional centers, corridors and neighborhoods work in concert to create the San Antonio we envision over the coming decades. In addition, 12 prototype place types offer examples of development models that can build upon and protect existing and future community assets while also creating places that are livable, inclusive and sustainable.

Policy guidance in the Comprehensive Plan is based on nine topical Plan Elements that address topics including: city growth patterns, transportation, housing, economic competitiveness, natural resources and the environment, the military, and historic preservation and cultural heritage, among others. Each Plan Element chapter includes an overview of key issues and challenges specific to the element and provides a set of goals and policies that set the direction for how our community will respond to or address the challenges before us.



SA Tomorrow Sustainability Plan

The Sustainability Plan focuses on the three pillars of sustainability (economic, environmental, and social) and is the roadmap for both the community and the municipal government to achieve the overall vision of a sustainable San Antonio as an inclusive and fair community with a thriving economy and a healthy environment. The Sustainability Plan highlights seven focus areas and five cross-cutting themes. Each focus area has its own vision, outcomes, strategies and measures of success. The cross-cutting themes identify and highlight key priorities. Additionally, these cross-cutting themes were considered and integrated into each of the major components and elements of the SA Tomorrow Comprehensive Plan and the Multimodal Transportation Plan.



SA Tomorrow Multi-Modal Transportation Plan

The Multimodal Transportation plan is a dynamic, balanced, and forward-thinking plan for all modes of transportation, including cars, transit, bicycles, and pedestrians. It communicates the City's transportation strategy and serves as a tool to analyze transportation priorities to best meet community goals. The City worked with stakeholders, partner agencies and the larger community to develop a plan that builds upon and operationalizes SA Tomorrow Comprehensive Plan goals and policies, incorporates all modes of transportation and recommends a sustainable, safe and efficient transportation system that can support the new residents, housing and jobs anticipated for our community over the coming decades.

SA Tomorrow Sub-Area Planning

Comprehensive Planning Program

The Comprehensive Planning Program (CPP) is the city's coordinated approach and process for city planning. It provides the rationale and goals for the city's long-range development efforts and contains three main service components: Building Capacity, Comprehensive Planning, and Implementation. The SA Tomorrow process identified several changes to be incorporated in the CPP, including new planning approaches and geographies. The intent of the revised hierarchy is to ensure that planning at all levels in San Antonio is completed in an efficient and effective manner with meaningful participation and buy-in from neighborhoods, property owners, business owners, partner agencies, major institutions and other key stakeholders.

While the Comprehensive Plan is the overarching planning and policy document for the City, there are four other planning levels including: Regional Plans (developed in collaboration with partner agencies to guide regional and multi-jurisdictional services and/or infrastructure investments); Citywide Functional Plans (directing specialized components of city planning such as transportation, economic development, housing, natural resources and sustainability); Sub-Area Plans (providing detailed strategies for specific geographies, such as regional centers, corridors, and neighborhood groupings, aligning them with higher level plan); and Specific Plans (addressing smaller scale geographies and focused on implementation).

The Texas A&M - San Antonio Area Regional Center Plan that this Atlas addresses is part of the Sub-Area planning program described in more detail below.

San Antonio Sub-Area Planning

Following adoption of the Comprehensive Plan in August 2016, the Planning Department is working with communities to create Sub-Area plans for the 13 Regional Centers and 17 Community Areas that collectively cover the entire City of San Antonio.

Regional Centers are one of the major building blocks of San Antonio's city form and a focus of the overall SA Tomorrow effort. While most cities have one or two larger employment centers, we have 13. This provides challenges and opportunities. A major organizing principle of the City's Comprehensive Plan is to focus growth in these Regional Centers, building on the existing pattern of development. They are envisioned as new "places" where we live, work, and play. Each center is different and its development will be influenced by its existing uses. However, many of the centers are also well-positioned to develop as vibrant mixed-use places. They offer a variety of housing options and price ranges, allow higher-density and incorporate carefully designed and located amenities that will benefit both residents and employees of the center. San Antonio must focus its investment and infrastructure strategies on supporting and leveraging the unique identity and assets of each center.

Community Areas form the rest of San Antonio outside of the Regional Centers. These areas comprise many of our existing neighborhoods, grouped by geography and common history, challenges, and opportunities. By proactively directing a higher proportion of growth to our Regional Centers, we aim to limit the impacts of that growth on existing, stable neighborhoods. However, cities and neighborhoods are

always evolving, and we must plan to adapt to and leverage change for all our existing and future residents by creating complete neighborhoods and communities that provide a full range of amenities and services, a variety of housing and transportation choices, and opportunities for employment, shopping, education, and recreation.

The Regional Center and Community Area Plans will address the following topics based, in part, on the existing conditions identified in this Atlas: Land Use; Parks and Open Space; Economic Development; Housing; Mobility and Infrastructure; Placemaking and Urban Design; and Policy and Investment Priorities.

SA Tomorrow SA Corridors

SA Corridors is a collaborative effort to help lay out the future of our city in the most equitable, sustainable, and efficient way possible. It is one of the first steps in implementing SA Tomorrow as well as VIA's Vision 2040 plan, which both identified corridors as areas where future growth should be concentrated where appropriate. SA Corridors objectives overlap with and support the ongoing Sub-Area planning efforts. The plan develops a recommended future land use map and plan for 12 corridor areas, respecting existing small area and neighborhood plans while helping to implement SA Tomorrow. In addition, SA Corridors recommends proper regulations and incentives are in place to realize SA Tomorrow's vision for growth and economic development around VIA's transit investments, and to help bring about new and more transit-supportive types of development that meet a range of desires and incomes.

Three Types of Regional Centers

The 13 regional centers are grouped in three categories based on analysis of their existing conditions, unique traits and potential growth capabilities. It is important to note that they are not homogeneous places. Although they cover large areas, each one includes multiple place types, urban forms and land uses.



Activity Centers

These areas have high concentrations of people and jobs in a mixed-use environment. They should be highly walkable and well connected by multiple types of transportation and transit. They should have an even mixture of jobs and housing and contain amenities that support residents, workers and employers within the centers and also throughout the city. Many are home to our educational, entertainment and cultural institutions.



Logistics/Services Centers

These areas have superior connectivity for the movement of goods and people including air, freight/rail and roadway transportation. This positions them as launching points for the city's exports and imports. These centers have large, coordinated areas of single uses, and concentrated nodes of mixed-use, with more jobs than residents. They provide goods and service to support businesses and residents adjacent to the center.



Special Purpose Centers

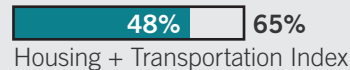
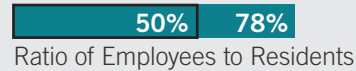
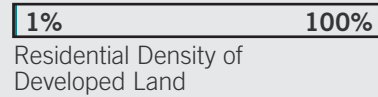
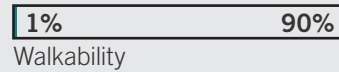
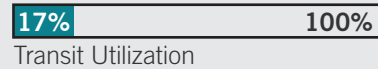
These areas have large employers, institutions and/or concentrations of similar types of employment. These centers typically require a barrier or buffer to separate their specialized activities from surrounding areas. They mostly contain primary employers and supportive services and amenities.

Regional Centers are one of the key building blocks of our city's future. In order to leverage their potential to help absorb San Antonio's projected growth we need a clear vision and strategic plan for each. These regional center plans need to refine each center's boundaries, identify areas of change and stability, and develop a detailed land use plan that prioritizes infrastructure, policy and program improvements. While these centers should promote higher-density, mixed-use development, not all areas within a regional center are recommended for this type of growth. For example, existing historic districts and neighborhood conservation districts are not recommended for higher-density development and should receive enhanced protection to prevent this. Our historic and conservation districts are some of our city's greatest assets and our development policies should protect them. Regional center plans must be respectful of these special areas when defining development opportunities.

Regional Center Area Profile

TEXAS A&M - SAN ANTONIO AREA REGIONAL CENTER PROFILE FROM THE SA TOMORROW COMPREHENSIVE PLAN

Existing & Aspirational Scores



Strengths: The Texas A&M University - San Antonio regional center benefits from two strong employment anchors, as well as ample land for future redevelopment.

Population (2015 estimate): 1,070

Households (2015 estimate): 308

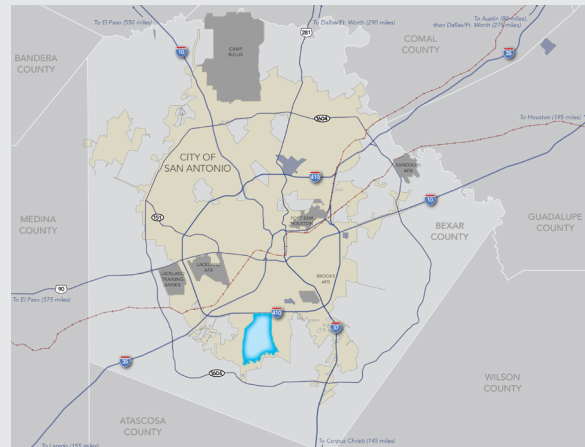
Single Family to Multifamily Housing Units Ratio: 0.78

Employment (2013 estimate): 3,800

Largest Industries (by employment): Transportation Equipment Manufacturing, Education

Acres: 7,240

Developed Acres: 5,260

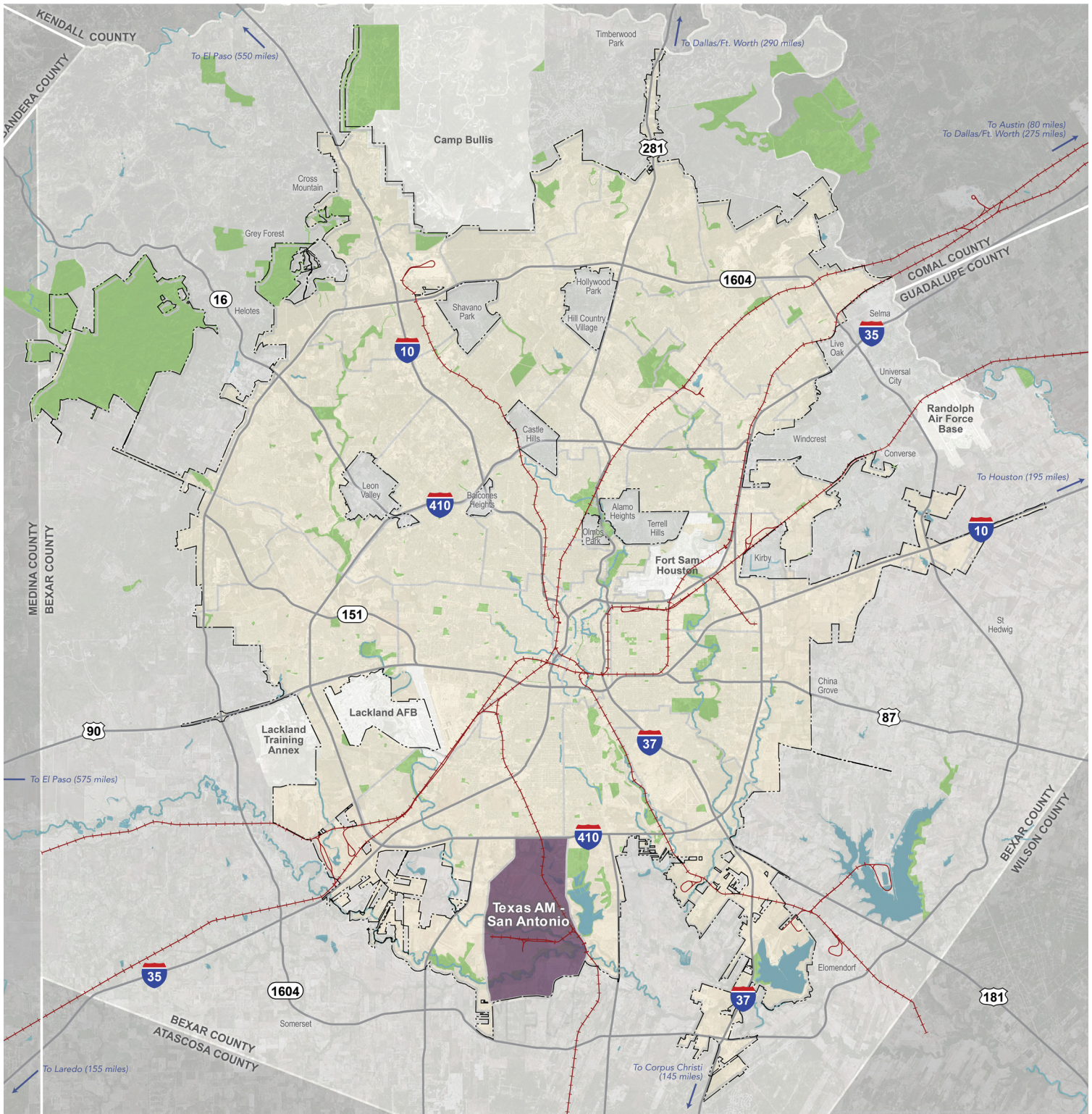


Anchored by the Toyota manufacturing complex and the new Texas A&M University-San Antonio campus, this is one San Antonio's emerging regional centers. The Toyota assembly plant employs over 3,000 and supports 23 related nearby suppliers. The university currently has 4,500 students and plans to grow to 25,000 students by 2025 on its 700-acre campus. Significant opportunities exist to develop vibrant mixed-use neighborhood serving the university and the south side of the City.

Tasks Ahead: Planning for this regional center should focus on increased connectivity and other infrastructure and investments that will catalyze the expected residential and employment growth.



HafenCity urban regeneration project in Hamburg, Germany is adding a diverse mix of uses and vibrant open spaces to an area that had limited infrastructure and amenities.



SA Texas A&M-
San Antonio
TOMORROW Area

PLAN LOCATION

- LEGEND**
- City Boundary
 - Major Highway
 - Rail Line
 - Park or Open Space
 - Water Body
 - Regional Center Boundary



History of the Texas A&M - San Antonio Area Regional Center

The Texas A&M– San Antonio Area Regional Center has a rich past with Native American history and connections to the San Antonio missions and Spanish and Mexican-era land grants. Among the many historic tracts of land in the area is one of the oldest ranches in Texas, the former JLC Ranch, which had been family-owned for more than 200 years since the original grantee, Don Juan Ignacio Perez de Casanova, a Canary Islander, began raising cattle on the land in 1794. The 2,000 plus acre property was sold to the Toyota Manufacturing Company in 2003. The Toyota Company donated 600 acres of the former ranch to the City of San Antonio to create green space along the Leon Creek and Medina River. Institutions, such as the Land Heritage Institute, serve to promote lifelong education for students of all ages by providing interactive experiences along the Medina River with historic, cultural and environmental landscapes to conserve and promote the uniqueness of the area. The Regional Center is named after its largest institution, [Texas A&M University - San Antonio \(A&M-SA\)](#), which was established in fall 2008, and today serves over 6,500 students.

Texas A&M University - San Antonio was created to address the educational needs in the south side of San Antonio. In 1997, the late Texas Senator Frank Madla began requesting proposals to establish an institution of higher education. The Texas A&M University System responded with a proposal to create a System Center on the Palo Alto Community College campus. In the fall of 2008 the system officially began classes for 126 students in seven academic programs. By the year 2007, the Verano Land Group donated 694 acres of land just South of Highway Loop 410, on the city's south side, for the construction of a new campus. In 2009, student enrollment had increased more than 62%, which required additional facilities to manage the growth. Later that year, Senate Bill 629 was passed to establish Texas A&M University - San Antonio as a stand-alone institution. In 2017, the university adopted its most recent Master Plan which is described in more detail in the “Previously Adopted Plans” section on page 16.



Texas A&M - San Antonio Area Infrastructure and Institutions

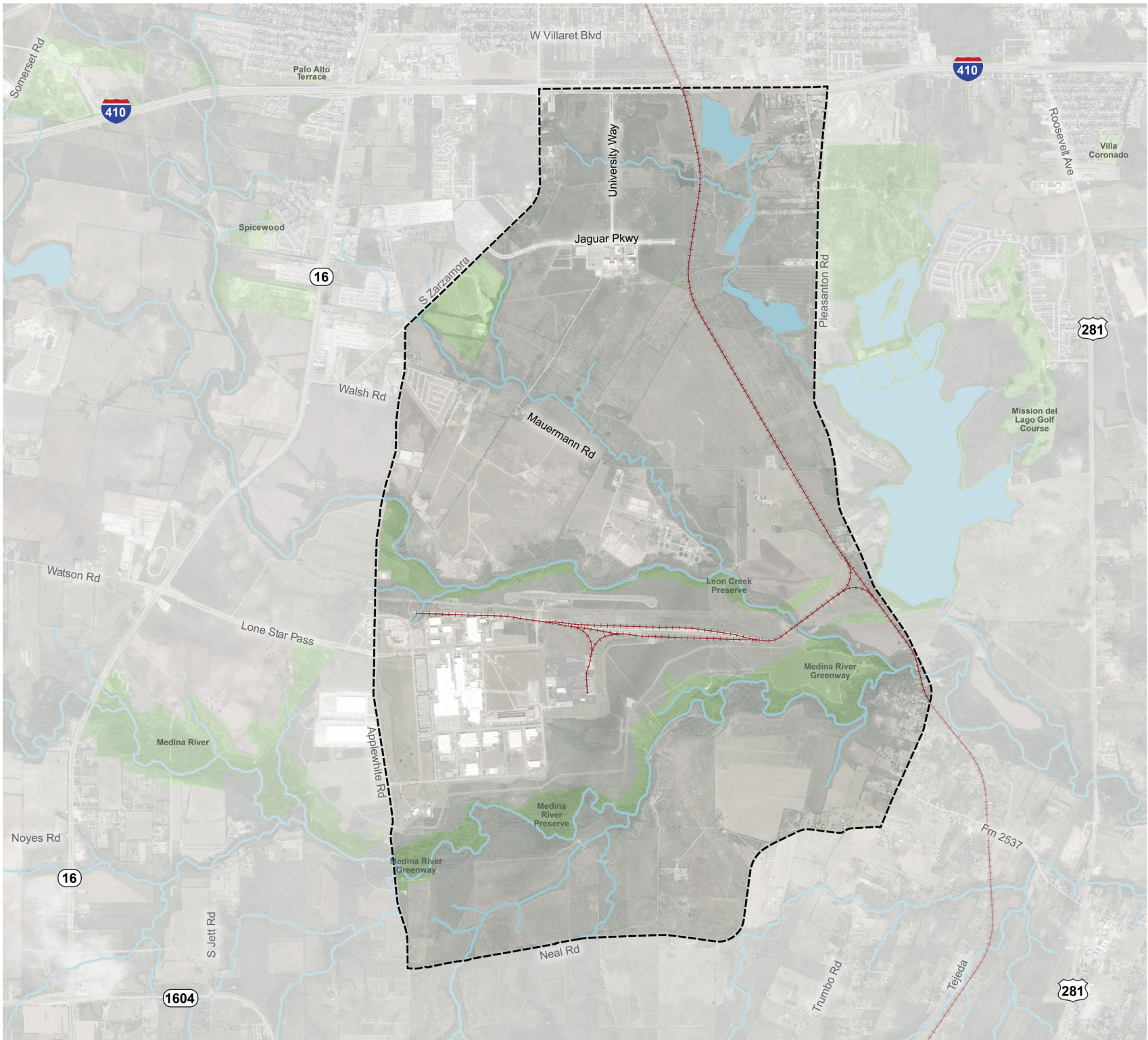
Major Landmarks and Infrastructure

The only highway that exists within the Texas A&M San Antonio Plan boundary is Interstate Loop 410, which runs east and west along the northern boundary of the Regional Center. Loop 410 intersects Interstate 37 (I-37) approximately seven miles to the east, and Interstate 35 (I-35) approximately five miles to the west before turning northwest toward JBSA-Lackland Air Force Base and U.S. 90. This segment of Loop 410 between I-35 and I-37 generally demarks the transition from urban to rural parts of south San Antonio. Loop 410 connects to the University's main road, University Way, and also serves as a major transportation route for students, employees and visitors to the campus. Loop 410 also provides an efficient route for large trucks and equipment to be delivered to the Toyota Manufacturing Plant and the other industrial uses along Applewhite Road.

The Leon Creek Water Recycle Center is one of three water recycling facilities and managed by the San Antonio Water System (SAWS) and it is located on Mauermann Road in the center of the plan area. San Antonio has the largest direct recycle water delivery system in the nation. More than 130 miles of pipeline delivers recycled water for use by parks, commercial and industrial sites as well as the San Antonio River Walk. Water recycling, or the effective treatment and reuse of wastewater, protects public health, the natural environment and wildlife, downstream water supplies and water sources for recreational uses such as golf courses.

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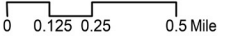




Texas A&M - San Antonio
REGIONAL CENTER

STUDY AREA

- Regional Center Area Boundary
- Adjacent Regional Center or Community Area
- Public or Private Park or Open Space
- River or Stream
- Railroad Line



Demographics and Economic Profile

Area Overview

The Texas A&M – San Antonio Regional Center is located in southern edge of San Antonio south of I-410, between Applewhite Road and Pleasanton Road. The focal points of the regional center are the Texas A&M – San Antonio Campus and the Toyota Manufacturing plant. The area was designated as an emerging regional center in the SA Tomorrow Comprehensive Plan due to the two major economic assets located in the center. The area is largely undeveloped and this regional center planning effort presents the opportunity to develop a long term vision to leverage these economic assets and create a vibrant employment center on the south side of San Antonio.

Overall, the Texas A&M – San Antonio Regional Center:

- Has a small population, accounting for just 0.1% of the City’s population
- Has attracted higher concentrations of family households and a higher average household size than the region overall
- Has a significantly younger population than the rest of San Antonio and Bexar County
- Has lower educational attainment and income levels than the region as a whole
- Lacking employment uses aside from the two major anchors
- Has a large amount of undeveloped plan to accommodate future growth

People

Total Population (2018) | **1,430**

Total Households (2018) | **411**

Population and Households:

In 2018, the population of the area was approximately 1,400, with just over 400 households. After attracting new housing development in the 2000’s and growing at 7.8% annually from 2000 to 2010, the population growth in the Texas A&M Regional Center slowed to about 3% annually since 2010, adding just under new 300 residents. The characteristics of households in this Center differ from the City overall, with 79% of all households defined as “family”, while 65% of households in the City are family households. The average household size in the Texas A&M Center is 3.23.

Age:

The Texas A&M Regional Center’s population is substantially younger than the regional population overall; the median age in the area is 25.9 years, compared to 34 in the City and 35.4 in the MSA, and 48% of the population is under age 20, compared to 36% in the City and 35% in the MSA. The population of “Millennials” (born between about 1980 and 1995) in the Center is similar to the City and the MSA, with 23% of the population

aged 20 and 34. The senior population in the Center is lower than the region, with only 7% of the population over age 65, compared to 13% in the City and 14% in the MSA.

Race and Ethnicity:

The population of the Texas A&M Regional Center is 87% Hispanic, higher than the 65% in the City and 56% in the MSA, and 76% White, higher than the 71% in the City and 74% in the MSA. The area has a Diversity Index score of 54 – measured from 0 to 100, this number represents the likelihood that two random persons in the same area belong to different race or ethnic groups. The City and MSA both have a Diversity index of 72. This indicates that the Texas A&M Regional Center is more ethnically segregated than the City as a whole.



Race and Ethnicity
87% Hispanic-origin



Median Age
25.9 years
34.0 years - City of San Antonio Average



Annual Population Growth | 2010-2018



Demographics and Economic Profile

Income:

Personal and household income in the Texas A&M Regional Center is lower than the region overall. The average household income in the Center is \$58,800, compared to \$70,000 for the City of San Antonio and \$80,200 for the MSA. Median household and per capita incomes in the Texas A&M Regional Center are also lower than the larger surrounding areas.



Average Household Income

\$58,800

16% lower than City of San Antonio

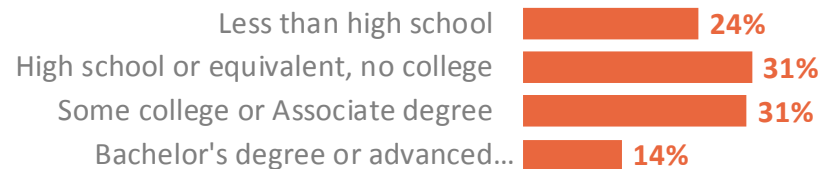
Education:

The education of the population and workforce in the Texas A&M Regional Center varies from the region overall. Of the area population aged 25 and older, 25% have less than a high school diploma (compared to 18% in the City and 15% in the MSA), and only 19% have an Associate's, Bachelor's, or Graduate/Professional degree (compared to 34% in the City and 36% in the MSA).



Education

45% of workers have some college or higher



Demographics and Economic Profile

Housing

There is a small, existing housing stock in the regional center as the center is just starting to develop residential uses. There are two residential sub-divisions of single family homes and a scattered mix of single family homes on larger, rural lots. Single family homes for sale in the within the subdivisions are listed at \$150,000 to \$250,000 according to Zillow.

There is only one apartment property in the Texas A&M Regional Center – Rosemont at University Park. This 240-unit building was constructed in 2006. The average rental rate for an apartment unit is \$787 per month, or \$0.82 per square foot. This is less than the Countywide average of \$952 per month, or \$1.11 per square foot.

The Texas A&M Regional Center has a lower proportion of owner-occupied housing units than the region - 43% of units are owner-occupied (compared to 53% in the City and 63% in the MSA), and 57% are renter-occupied (compared to 47% in the City, 37% in the MSA).

A large, master-planned community, Verano, has been planned and had received development approvals for 1,800+ acres between Loop I-410 and the Texas A&M-San Antonio Campus. The project is planned for 5,700 residential units and 3 million square feet of commercial and industrial development. The development has stalled and no development has occurred yet. The Texas A&M-San Antonio Campus has comprehensive master plan created in 2007 (modified in 2017) with an assumed multi-year/decade build-out. As part of the master plan, a student residential village is planned with capacity for 10,000 beds. Currently, there is no student housing on campus.

Housing targets:

The regional center is forecast to grow between 1,700 and 6,500 new households from 2010 to 2040. The estimated housing range is dependent largely on the future prospects of the Verano area. Development activity in the south San Antonio area indicates demand that supports the lower end of the estimate of 1,700 new housing units, however a major master planned development could greatly impact demand and the higher end estimate assumes the build-out of the Verano development and attraction of additional housing development in the area.



Average Household Size

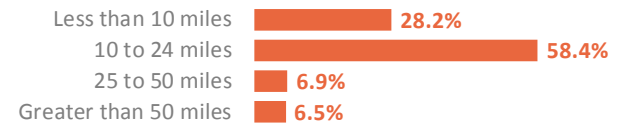
3.23 persons

2.71 - City of San Antonio Average

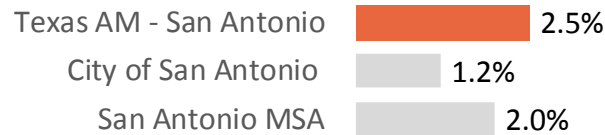


Commuting Patterns

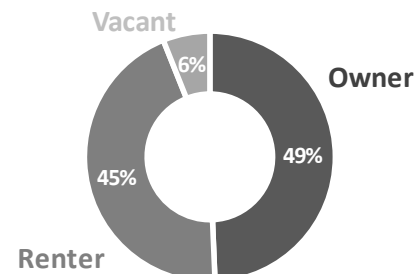
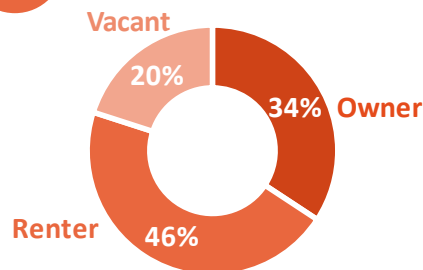
Less than 1% of workers live in the center



Annual Household Growth | 2010-2018



Housing Tenure



Demographics and Economic Profile

Total Employment (2018) | **3,800**

Employment

The Texas A&M- San Antonio Regional Center was estimated to have approximately 3,800 jobs located in the center. The estimated 2,600 jobs at the Toyota manufacturing plant account for 68% of jobs in the center. As described previously, the two major employers in this area are the Toyota manufacturing plant and Texas A&M – San Antonio. Employment in the Texas A&M Regional Center is largely in higher paying jobs; 65% of area jobs have earnings greater than \$40,000 per year (compared to 37% in San Antonio).

The Texas A&M-San Antonio campus master plan calls for between 4 million and 10 million gross square feet of development at build-out. The campus currently has enrollment of 6,500 students and faculty and staff of approximately 600 employees with plans to grow to near 10,000 students by 2021 and 12,500 students by 2025.

Most of the people employed in the Texas A&M Regional Center do not live in the area – nearly 100% of workers commute in from other places. Most workers come from relatively close by, with 28% commuting less than 10 miles, and another 58% commuting between 10 and 24 miles. Similarly, most people living in the Texas A&M Regional Center do not work in the area – only 3% of residents are employed in the area, while 97% commute out to other locations.

Commercial and Industrial Development

Office

There is no office space located within the Texas A&M Regional Center.

Retail

The only retail property within the Texas A&M Regional Center is an 8,000 square foot day care.

Industrial

The Texas A&M Regional Center has just under 2 million square feet of industrial space. A 1.5 million square foot Toyota plant was completed in 2006 which more than quadrupled the Center's industrial inventory. There is no vacant industrial space within the Center; this 0% vacancy rate is lower than the 4.8% rate in the County. Historical rent data is not available for this area.

Hotel

There are no hotel properties within the Texas A&M Regional Center.

Employment Targets:

The Texas A&M-San Antonio Regional Center is forecast to grow by approximately 20,000 jobs from 2010 to 2020. The vast majority of this growth is driven by the build-out of the Texas A&M – San Antonio campus and expected supporting industries.



Largest Employers

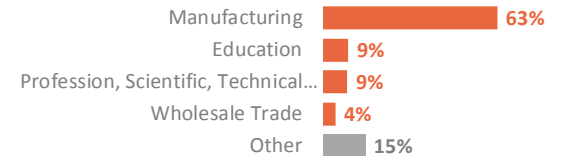
Toyota

Texas A&M – San Antonio



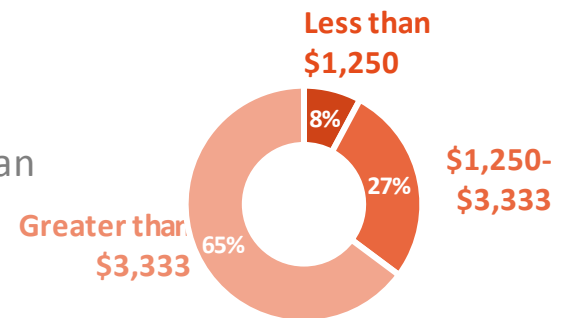
Largest Employment Sectors

Manufacturing and Education



Wage Distribution

65% earn more than \$3,333 per month



Previously Adopted Plans

Heritage South Sector Plan

The Heritage South Sector plan gets its name from the stakeholders who helped develop the plan, which identified the area's rich natural, archeological and historic resources as assets to the area. The plan area encompasses approximately 197 square miles of land in the City of San Antonio and Bexar and Wilson counties. Generally, its boundaries are Loop 410 to the north; I-35 to the west; the Extraterritorial Jurisdiction (ETJ) boundary on the south, and Hwy 181 to the east. The Texas A&M – San Antonio Area Regional Center is located in the middle of the Sector Plan area. The sector plan identifies 12 different land uses throughout the area and was adopted by City Council on September 26, 2010.

The Heritage South Sector Plan's vision is to encourage economic development, integrate urban and rural life through planned development with safe neighborhoods and preserve the history, values and natural resources of the area. The Plan is separated into six chapters including an introductory chapter, a chapter dedicated to describing the plan area, a sector plan element chapter, a chapter on the City South Community Plan, an Action Plan, and an appendix. The Sector Plan Elements Chapter provides information on the nine elements of the plan.

The Transportation element addresses the community's desire for a comprehensive transit system.

The Utilities and Infrastructure element provides goals and strategies to support land use policies, developing renewable energy sources, and floodplain management.



The goals and strategies of the Housing element reflect the community's desire for a diversity of housing stock.

The Economic Development element recognizes the possibility for sustainable growth through "green" technologies, premier education, agricultural activity and capitalizing on eco-tourism.

The Community Facilities element envisions accessible, multi-use community facilities that include parks and recreation facilities, public libraries, hike and bike trails, and sport facilities.

The Education element addresses the community's desire that schools and public libraries be an integral part of the community, and the goals and

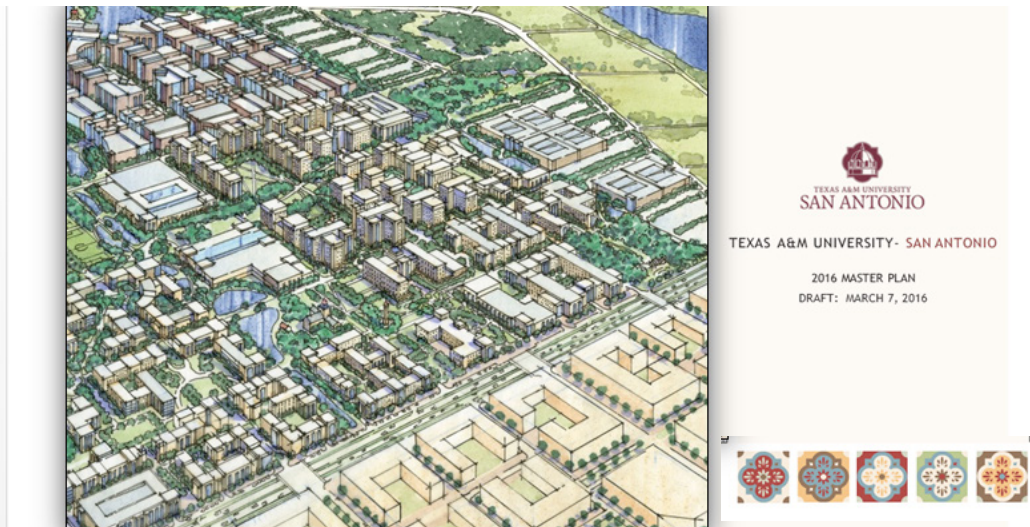
strategies range from early childhood development to workforce development.

The Parks and Natural Resources element describe the areas natural resources as an asset to the community in terms of aesthetics, recreation, education and economic value, the goals and strategies support preservation and linear parks.

The Historic Preservation element highlights the importance of historic preservation efforts in the area.

Lastly, the Land Use and Urban Design element provides a wide variety of land uses throughout the sector in respect to preserving "natural tier" areas such as floodplains, rivers, lakes and creeks.

Previously Adopted Plans



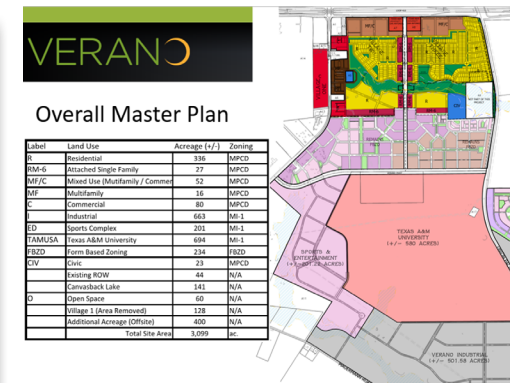
Texas A&M San Antonio Campus Master Plan

The 2017 Texas A&M University - San Antonio Campus 2017 Master Plan is the most recent campus plan. It is a guide to campus development to accommodate growth while balancing academic programming, environmental, financial, and cultural, and other considerations. Over the next several years, the University anticipates growing by more than 6,000 students. The Campus Master Plan accommodates the this increased enrollment by expanding existing academic and administrative facilities; ; adding on-campus student housing, recreation, and student-life functions; and identifying critical infrastructure projects as well as including parking.

The 2017 Campus Master Plan envisions a total build-out of campus to that will eventually

accommodate more than 50,000 students. Higher density and more efficient connections to regional, ecological, and recreation systems are featured, as well as connectivity with the regional transportation infrastructure.

The plan identifies six districts within the University which are connected by pedestrian-friendly pathways and eco-corridors. The districts include: an Academic Core District, a Student Housing District, a Student Life District-which would contain facilities such as dining, a student union and central library, a Student Recreation District-which supports recreation and intramural fields, an Athletics District that would accommodate stadiums and fields for competition, and a Research District that would support future development for academic endeavors. The plan also incorporates architectural and landscape design elements that respect San Antonio and South Texas' cultural heritage.



Verano Master Development Plan

The Verano Master Development Plan (MDP) is an approximately 2,700 acre redevelopment plan that included nearly 700 acres for the Texas A&M University San Antonio (A&M SA) Campus. The Verano MDP spurred the development of the A&M SA campus as well as planned for an entire mixed-use community developed around the campus. The full build out of the plan called for the area to be home to 30,000 residents and 30,000 students. The MDP's potential led to the City of San Antonio creating the 30 year Tax Increment Reinvestment Zone (TIRZ) for the area. The overall plan includes a town center, anchored by a commuter rails system, and three "villages" surrounding the campus with housing, green spaces, plazas, trails, and neighborhood retail and office uses. In addition to retail and office buildings that are designed to be walkable, the Verano MDP will also provide sports and entertainment facilities, the Verano Research and Technology Center, as well as an aging-in-place and healthcare district. The MDP is designed to create a sustainable balance of social wellbeing, economic vitality and ecological responsibility.



Natural Systems

Two watersheds exist within the plan boundary. The Medina River Watershed covers the northern and southern portions of the regional center, and the Leon Creek Watershed exists in the middle of the plan boundary.

Medina River Watershed

The Medina River Watershed is served by three tributaries. Most notable is the Medina River, located on the south end of the plan area, mostly within the Medina River Natural Area. The Medina River's source begins in the Edwards Plateau in northwest Bandera County and courses 120 miles. The area is lined with natural landscape rich with pecan and bald cypress trees near the river, and cactus and honey mesquite in the upland trails.


The tributaries on the north end of the plan boundary feed into nearby Mitchell Lake, which is located just outside the plan boundary to the east. Mitchell Lake is a 600 acre lake that is also home to the Mitchell Lake Audubon Center which is a preserved open space that protects wildlife and native bird habitat, and serves as an educational tool for visitors and avid bird watchers.

Leon Creek Watershed

Leon Creek is a tributary stream of the Medina River. The source is northeast of Leon Springs in northwestern Bexar County. The creek runs southeast through the west side of San Antonio, and ends at the mouth of the Medina River, in the middle of the regional center plan area.

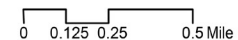
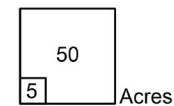
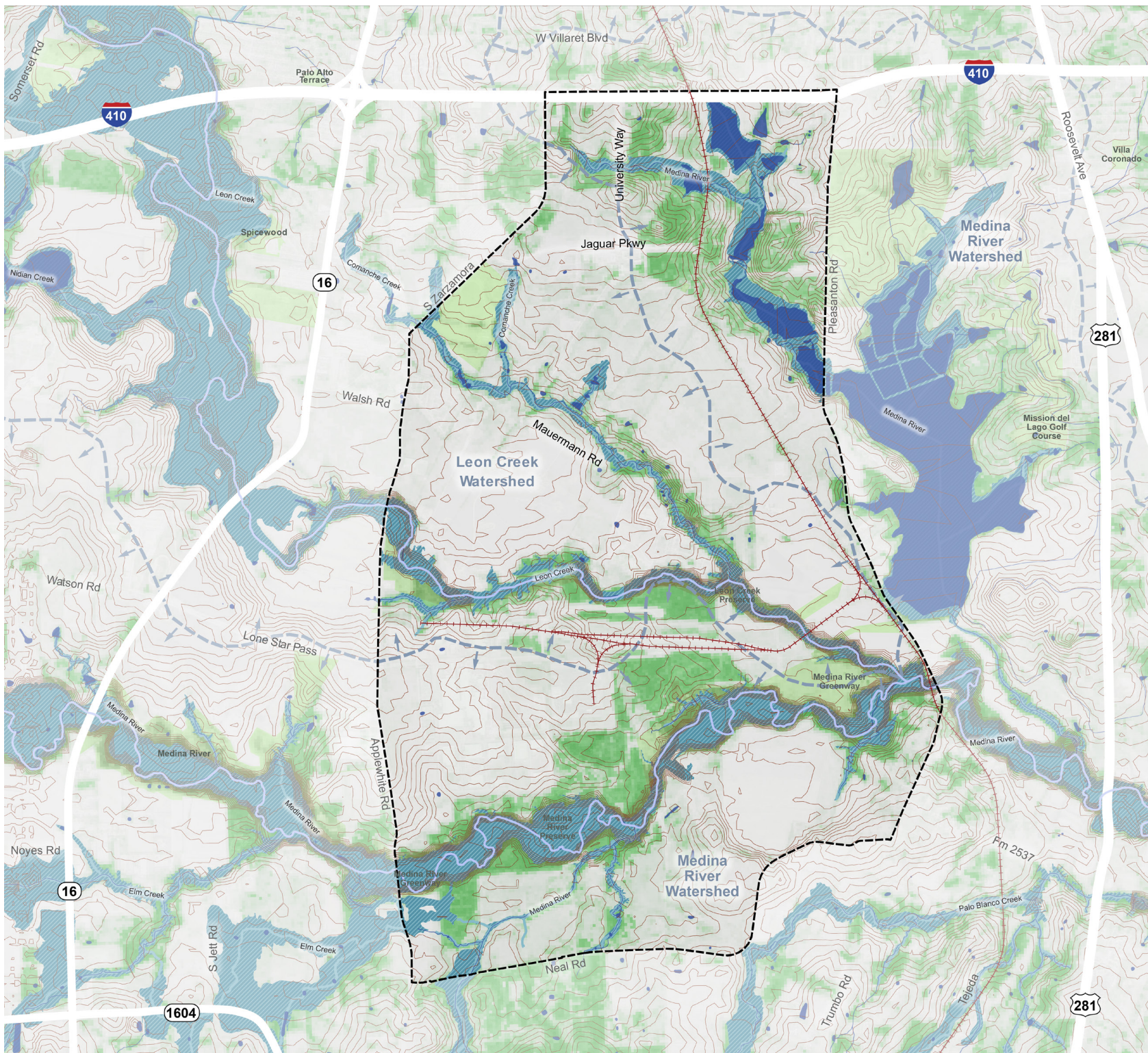


NATURAL SYSTEMS

-  Regional Center Area Boundary
-  Adjacent Regional Center or Community Area
-  Watershed Boundary
-  5-foot Contour Lines
-  FEMA 100-year Floodplain
-  Tree Canopy
-  Limited Water Quality Streams

WETLANDS

-  Riverline
-  Freshwater Wetland
-  Lake/Freshwater Pond





Land Use and Development Patterns

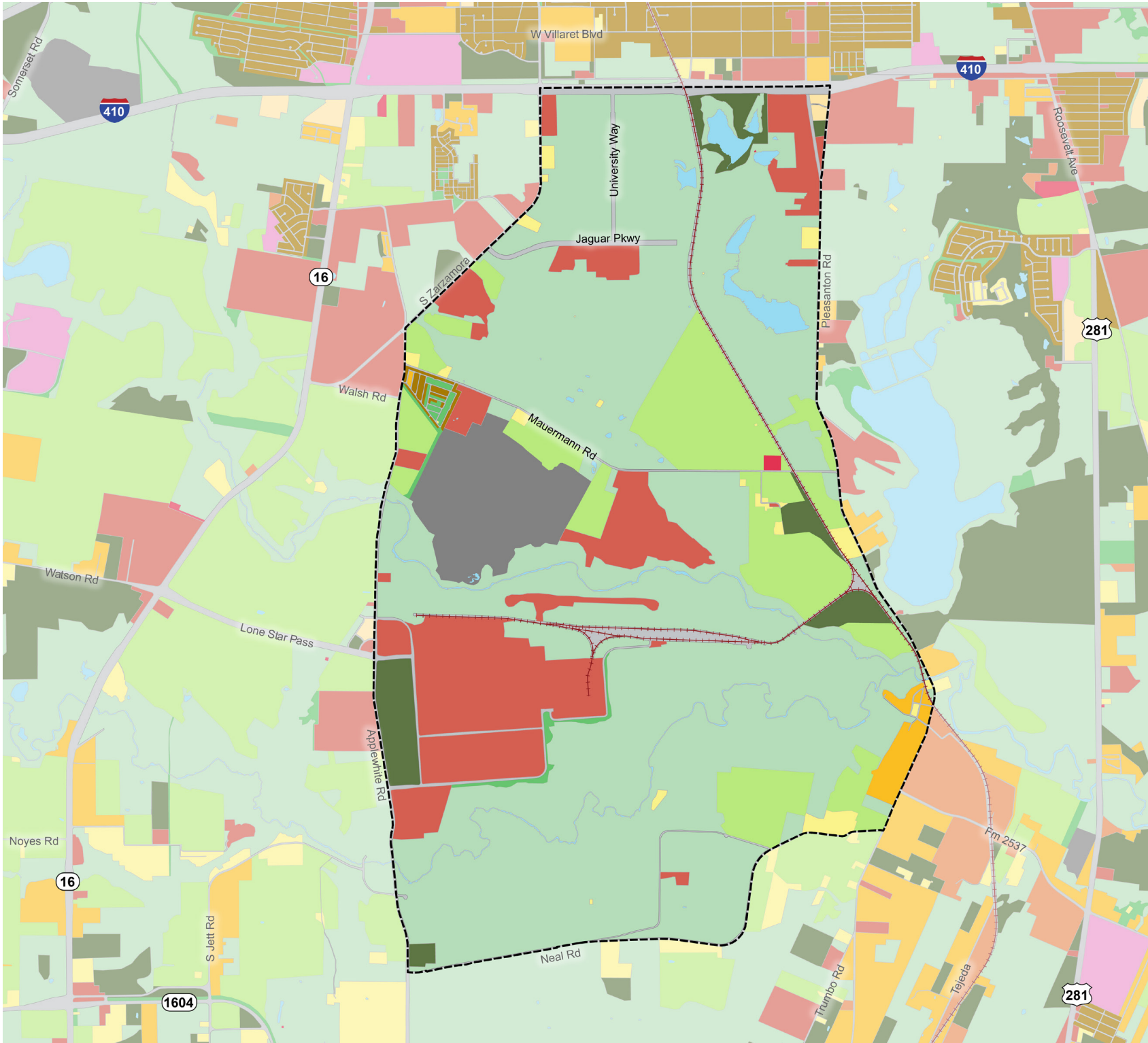
General Development Patterns

The Texas A&M San Antonio Regional Center area is largely undeveloped, and is 15 square miles in size. There are less than 300 parcels in the area, with only 190 property owners. Because the area has not seen very much development intensity, most of the tracts are still sized for agriculture, and most development is less than 15 years old, and is large-scale in nature (Toyota Motors, TAMU-San Antonio, etc.). Currently this planning area is predominantly serving employment and institutional purposes.

Loop 410, South Zarzamora and Applewhite Road are the major means of access to the TAMU-San Antonio Regional Center. There is limited infrastructure in the area, due to the significant amount of undeveloped land. There is a railroad running roughly north-south, with a switch yard at the Toyota Motors plant, servicing industry in the area. The railroad could become a barrier to future growth and development. Care should be taken to avoid this.

Land Availability

- The planning area is just over 15 square miles.
- There are less than 300 parcels in this area, with only 190 property owners.
- Over half of the land in the area has yet to be developed.
- There is a predominance of large size parcels, and most development to date has been large-scale in nature (Toyota Motors, TAMU-SA, etc.).

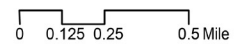
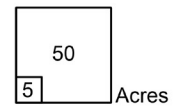


Texas A&M - San Antonio
REGIONAL CENTER

LAND COVER

- Regional Center Area Boundary
- Adjacent Regional Center or Community Area
- Service Utilities
- Industrial
- Extraction Mining
- Commercial
- Open Space Cultivated
- Open Space Urban
- Residential Dispersed
- Residential High Density
- Residential Medium Density
- Residential Low Density
- Residential Multi-Family
- Undeveloped Brush
- Undeveloped Meadow
- Water
- Transportation

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Land Use and Development Patterns

Distribution of Uses

Residential and Commercial

Only 2% of land area is used for residences, and less than 1% is used for commercial activities. Commercial uses such as convenient stores and gas stations exist along Loop 410 on the northern boundary of the plan area. The northeast corner of the plan area is the most developed, with a few commercial uses such as a feed store, tire shops, a hotel, and fast food restaurants along Pleasanton Road. The majority of single-family development in the area exists to the south east. This portion of the planning area is predominantly single-family use, with some mobile homes. The Rosemont at University Park is the only multi-family residential property within the plan area, and is located on the southeast corner of Pleasanton Road and Loop 410. As new development moves into the area, care should be taken so that residential options are provided for TAMU-San Antonio students and for people working at the various plants and facilities in the area. Retail and commercial development will also be needed.

Institutional

The northern portion of the plan area is the Texas A&M University San Antonio Campus. Although the campus owns nearly 700 acres, the majority of the University's property is undeveloped. Just as UTSA grew in phases over the past 40 years, the growth of TAMU-San Antonio will also likely occur in phases. Coordinating city planning efforts with campus planning efforts will ensure that the university and its surrounding community grow in a complementary and balanced manner.

Industrial

Toyota Manufacturing Plant and its other supporting uses, such as shipping and receiving facilities on Applewhite Road, are the primary industrial uses in the area. The presence of the Toyota Motors facility makes this a significant employment center for the City. The rail spur and switch yard provide valuable transportation infrastructure for this area.

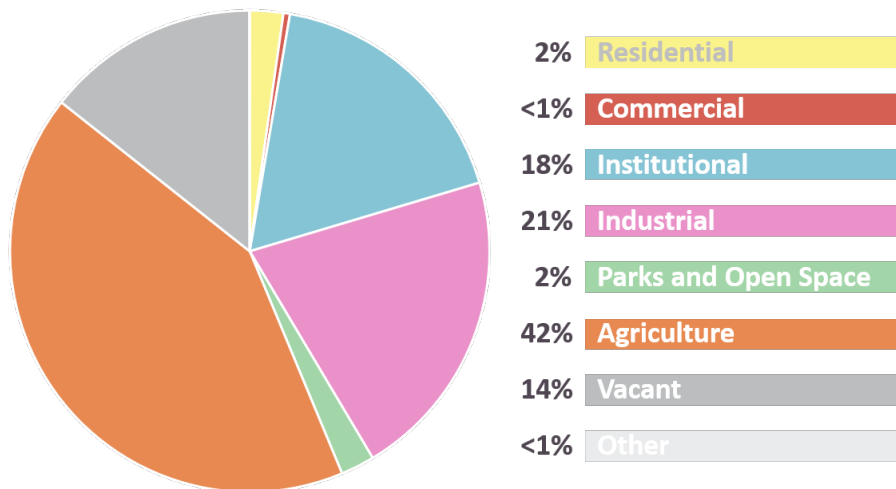
Parks and Open Spaces

The watershed of the project area, including Medina River, Leon Creek, Comanche Creek, and Mitchell Lake, will impact the form, intensity and configuration of future development. These riparian areas also provide opportunities for greenbelts and regional place-making. The Land Heritage Institute comprises 1,200 acres of privately managed open space for conservation and education along the southern edge of the planning area. It adjoins the Medina River. Prohibitive zoning districts such as "Resource

Protection" prevent the much of the area in close proximity to the river and creek to being developed for other than agriculture or low intensity uses.

Agricultural

42% of the land in this planning area is used for agricultural purposes, including ranches and farms. The predominant crops in Bexar County are hay, corn, wheat and sorghum, and this plan area covers part of the production area of Bexar County. Horse and cattle ranches, like Zarzamora Ranch, are also found in this area. Much of the existing infrastructure is in place to service rural uses. Palo Alto, Zarzamora, Mauermann and Neal Road function as farm roads today, supporting limited freight and passenger traffic. As development activity increases in this area, street and utility improvements will be needed to service more urban uses.

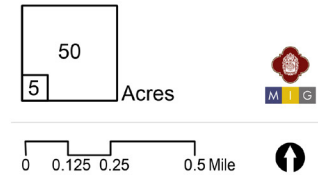
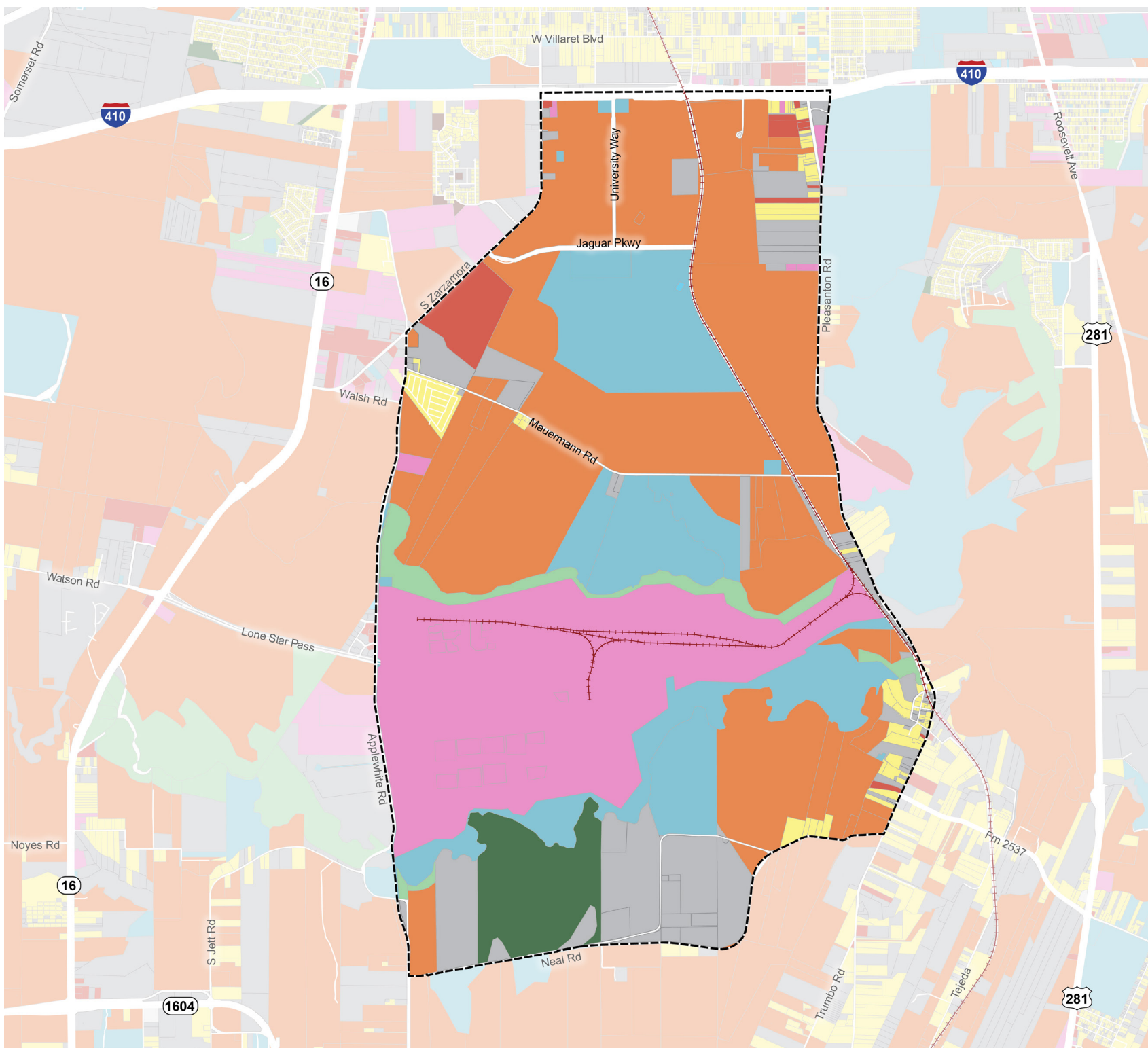




Texas A&M - San Antonio
REGIONAL CENTER

EXISTING LAND USE

- Regional Center Area Boundary
- Adjacent Regional Center or Community Area
- Institutional and/or Total Tax Exempt
- Industrial
- Commercial
- Agriculture
- Residential
- Open Space / Park
- Private Open Space
- Vacant
- Other / Not Specified





Mobility

The rural nature of the plan area lends itself to being predominately auto oriented. The plan area is largely spread out, and two roads, Mauermann and Neal roads connect the east and west boundaries of the plan area. Jaguar Parkway and University Way, on the A&M SA campus, are fully built out with side-walks on both ends as well as bike lanes and on-street parking, as well as clearly identified crosswalks.

Two VIA bus route serves Jaguar Parkway, on the A&M SA campus. The 44 bus departs Downtown San Antonio at Martin and Navarro Street and ends at Jaguar Parkway. The 520 bus departs Palo Alto College, north of loop 410, and ends at Jaguar Parkway.

There are no existing bike lanes within the plan area, with the exception of the aforementioned University Way and Jaguar Parkway. However, the 2011 Bicycle Master Plan calls for bicycle lanes on Zarzamora, and throughout the A&M SA campus. The Medina River Greenway has existing bike paths that run east and west close to the southern portion of the plan area.



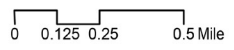
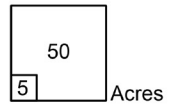
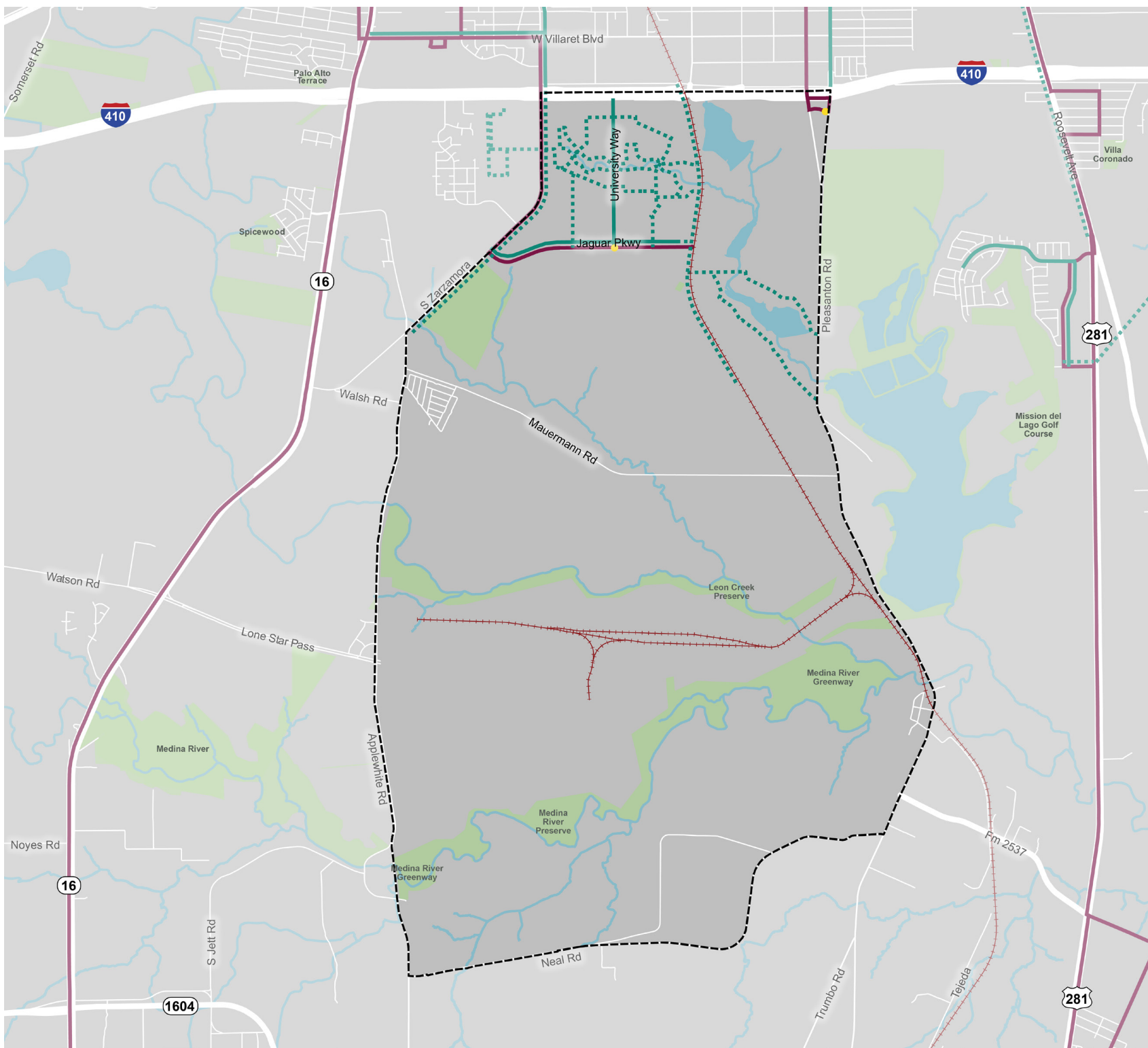


Texas A&M - San Antonio
REGIONAL CENTER

MOBILITY MAP

- Regional Center Area Boundary
- Block
- Rail Line
- Park or Open Space
- Stream
- VIA Bus Route
- VIA Bus Stop
- Existing Bicycle Trail, Lane or Route
- Proposed Bicycle Trail, Lane or Route*

* Acquired from the 2011 Bicycle Master Plan





Amenities and Access

Interstate Loop 410 is the most notable access point in the Texas A&M – San Antonio Area Regional Center. It is the highest capacity roadway in the plan area, and serves the northern boundary of the Regional Center. Although the southern portion of Loop 410 passes through predominantly rural areas, it connects these areas with the rest of the City of San Antonio, as well as I-37, I-35, and Highway 90. Applewhite Road is also a major roadway within the plan area. While it does not currently accommodate as much daily traffic as other roads within the city, it is the primary route for large trucks to access the Toyota Manufacturing Plant (via Lone Star Pass) as well as the other industrial properties within the plan area. Applewhite Road's ability to support 18-wheeler trucks and other heavy equipment is essential in ensuring the success of the Toyota plant as well as the other industrial uses in the plan area.

The Union Pacific and Burlington Northern Santa Fe Railroad companies share the railroad that exists on the western side of the plan boundary. In 2003, a second rail line was constructed that now runs east and west, to and from the Toyota Plant. The shared rail line creates competition between Union Pacific and Burlington Northern for freight, which keeps transportation costs low for Toyota.

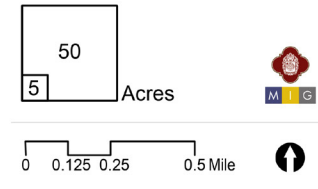
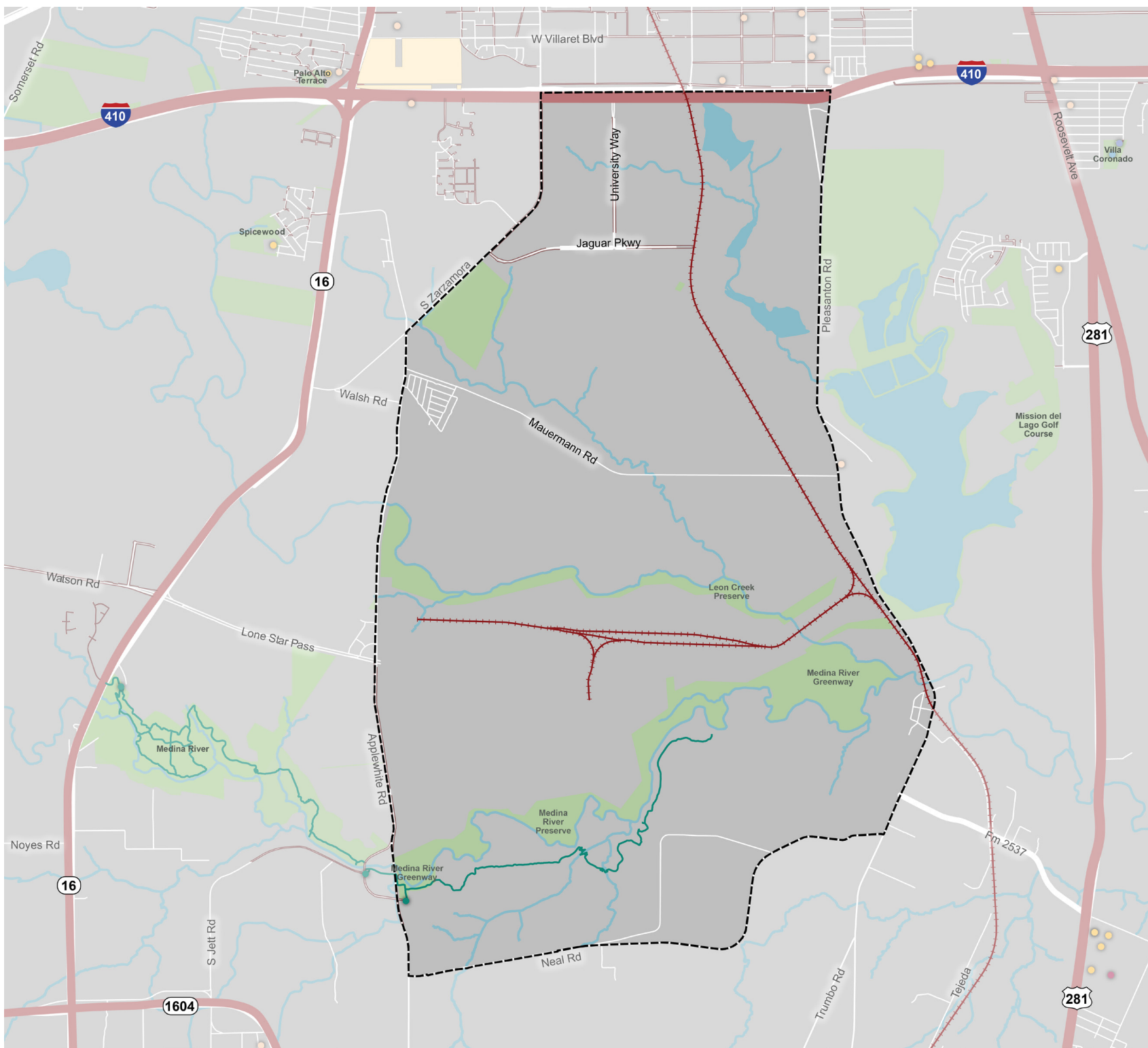
The Medina River Natural Area is the city's only natural area on the south side, located along the Medina River (mentioned above), the 511 acre property features 7 miles of trails, an interpretive trail representing the El Camino wagon trail, and group camping sites, and is also the only riparian natural area within the current natural areas system. The area is lined with natural landscape rich with pecan and bald cypress trees near the river, and cactus and honey mesquite in the upland trails.



Texas A&M - San Antonio
REGIONAL CENTER

AMENITIES AND ACCESS

- Regional Center Area Boundary
 - Adjacent Regional Center or Community Area
 - Park or Open Space
 - College Campus
 - Stream
 - Greenway
 - Public School
 - Place of Worship
 - Community Center
 - Recreation Center
 - Trailhead
 - Theater/Auditorium or Performing Arts Center
- Physical Barriers
- Major Road
 - Rail Line
 - Incomplete Sidewalks



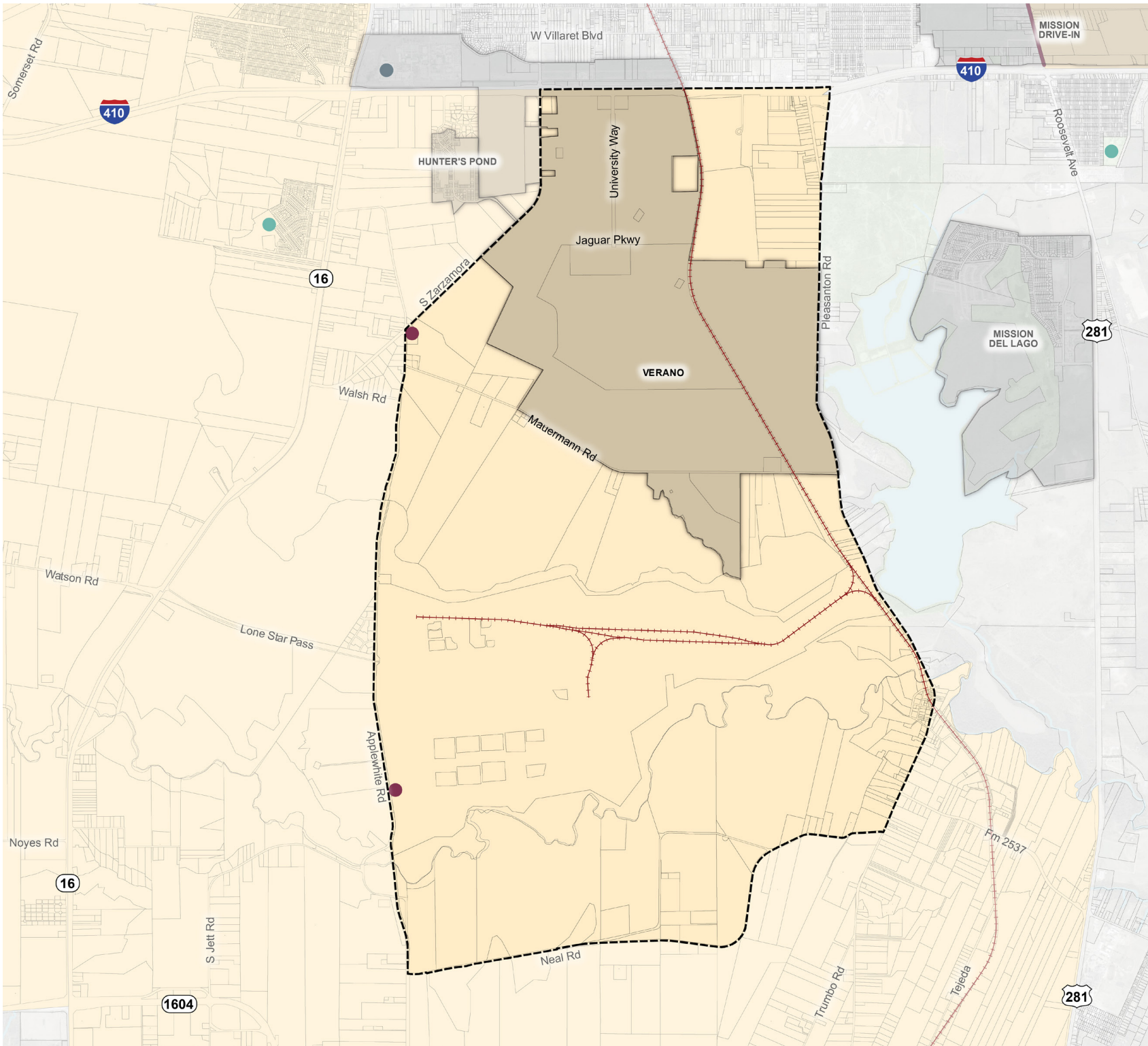


Public Investments

The Verano Tax Increment Reinvestment Zone (TIRZ) project was designated in 2007. The TIRZ includes 3,100 acres of the Verano site, and is located in City Council Districts 3 and 4, and Bexar County Precinct 1. The City of San Antonio would contribute approximately \$199 Million over the course of 30 years. The city's contribution to the TIRZ would be in the form of property tax revenue for public improvements. The TIRZ will reimburse costs associated with the public improvements for the project which includes: streets, stormwater drainage/retention, water, sewer, street lights, street signs, streetscapes, linear parks, parks/plazas, platting/zoning fees, storm water pollution prevention, park fees, drainage impact fees, off site drainage, sewer/water impact fees and environmental review. The city had anticipated infrastructure improvements to facilitate 140 single family homes through 2017, 200 multi-family

homes, 25 condo/townhomes as well as retail and office space, however no plans have been approved for the project and development has not occurred. As of December 2018, the property owner, Verano Land Group, is in negotiations to find buyers for the property.

The 2017 Bond Program includes two ongoing bond projects related to street improvements in the Texas A&M - San Antonio Area Regional Center. The first project will reconstruct and widen the Applewhite intersection south of Watson Road, including traffic signal modifications as appropriate. The project is scheduled to begin in August 2019 and to be completed within one year. The second bond project is to reconstruct the intersection of South Zarzamora Street and Applewhite Road. The project is estimated to begin in January 2020, and be completed within one year.

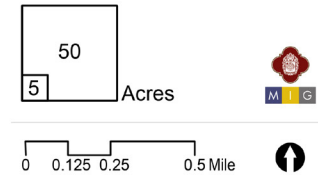


Texas A&M - San Antonio
REGIONAL CENTER

PUBLIC INVESTMENTS

- Regional Center Area Boundary
- Adjacent Regional Center or Community Area
- Parcels
- City Initiated Tax Increment Reinvestment Zone (TIRZ)
- Opportunity Zones

- Proposed 2017 Bond Program
- Street
 - Facility
 - Park
 - Street





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